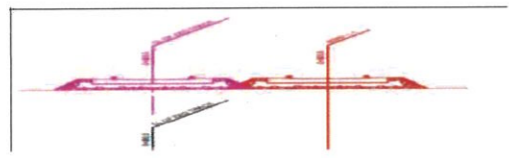


Name of the Work: "DESIGN AND CONSTRUCTION OF ELEVATED VIADUCT OF LENGTH 8.027 Km (CH: -0.675 Km to -0.050 Km & CH: 11.137 Km to 18.350 Km) INCLUDING RAMPS AND FORMATION IN EMBANKMENTS /CUTTINGS INCLUDING BLANKETING, MAJOR BRIDGES, MINOR BRIDGES, RUB, ROB, ROR, RETAINING WALL, SACRIFICIAL RETAINING WALL AND DRAINS, UTILITY DIVERSIONS OF AT-GRADE SECTION OF LENGTH 17.551 Km (CH: -0.964 Km to CH:-0.675 Km, CH: -0.050 Km to CH: 11.137 Km & CH: 18.350 Km to 24.425 Km) AND OTHER RELATED INFRASTRUCTURAL WORKS FROM BENNIGANAHALLI TO CHIKKABANAVARA, EXCLUDING STATION BUILDINGS, OF CORRIDOR - 2 OF BENGALURU SUBURBAN RAILWAY PROJECT (BSRP)".

Replies to the Queries raised by prospective bidders including Pre-bid meeting discussions held on dated: 11.01.2022

Bidders Queries after Corrigendum/Addendum and Pre-Bid queries and replies.


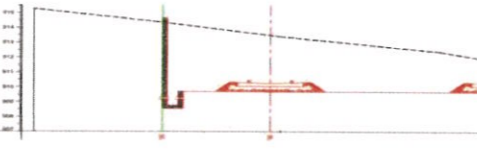
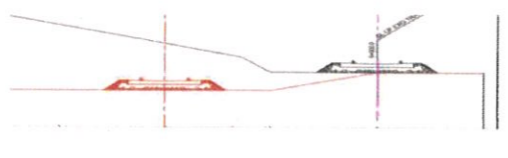
Running Sl No:	Reference as per Tender Document	Tenderer's Query	Reply/Clarifications by K-RIDE
	c) The intending Tenderer / firm/ company / Joint Venture should have executed all the components within last five financial years and Each component should have been executed in any one year (Any continuous 12 months).		
1	Clause: 3.2 (c) 1, The launching activity of Prestressed concrete superstructure of length of minimum 4.00 KM with Heavy duty crane and Launching Girder of Elevated Viaduct / flyover / bridge for Metro Railway/ Railway/ High Speed Railway /Regional Railway/ Light Railway/ Road Project having pile foundation. (In maximum up to Two contracts) AND	We request you to kindly remove the capping of one year (12 Months) for completion of 4.00 Km structures. Considering the normal time requirements for construction of such long length structures, it may not be prudent to propose 12 months completion period and this will restrict the healthy competition amongst the prospective bidders	The present tender condition prevails.
2	Clause: 3.2 (c) 2, Completion of at least one bridge/ viaduct involving launching of steel/ PSC girder for at least one span of 40 m or more for a Metro Railway/ Railway/ Regional Railway/ Light Railway/ Road over the Railway running track. (In One contract and this can be met through Nominated/ Identified subcontractor who has experience in this type of work) AND	We request you to kindly remove the capping of one year (12 Months). Considering the normal time requirements for construction of such long length structures, it may not be prudent to propose 12 months completion period and this will restrict the healthy competition amongst the prospective bidders	The present tender condition prevails.
3	Clause: 3.3 (I), KEY PLANT AND EQUIPMENT: Availability by owning at least 50% of the required following key and critical equipment's for this work and the remaining 50% can be deployed on lease/hire for all works provided, the relevant documents (Commitment agreements etc.,) for availability for this work are to be furnished:	For fulfillment of qualification requirement under this clause may please accept undertakings that "In case of award of work the contractor undertakes to deploy all Equipment" as per tender documents	The present tender condition prevails.
4	Clause: 3.3 (c'), LIST OF MINIMUM KEY TECHNICAL PERSONNEL: List of Minimum Key Technical personnel required for the work are as under and should be enrolled in company/ firm/Joint Venture under Employment register and document should be uploaded. The Contractor shall have a competent team of Managers, Engineers, Technical staff etc. so as to complete the work satisfactorily as per various requirements of the contract. The Key Positions not limited to (and in addition to other manpower requirement as given in the Tender document) and corresponding qualification and experience are as under:	For fulfillment of qualification requirement under this clause may please accept undertakings that "In case of award of work the contractor undertakes to deploy all Manpower" as per tender documents	The present tender condition prevails.
5	General Time Extension for bid submission	Considering the Vast & complexity of project involving design in Multi fold nature, the time for Bid preparation is not enough and date of submission shall be extended by 45 Days from the post issuance of Prebid Clarifications. Kindly Confirm	The bid submission date is amended as : Last Date and Time for receipt of Tenders Dated:21/02/2022, IST 15:00 Hrs. (Only electronic Tender permitted). Corrigendum-3 is uploaded for the same in Karnataka e-procurement portal and K-Ride website. ✓
6	Annexure Pb-19, Pg-792 to Pg-805 Structural design details	Drawings provided as annexure pb-19 are not clear. Please provide clear/readable drawings to better understanding.	PB19 drawings are RDSO reference drawings and it is also available in RDSO site. However, required drawings are enclosed as Annexure PB-1A. (CF, F2, F3) P. Anand Prakash
7	General- 31m Span for U-Girder	Generally U-Girder spans are designed for 28m, Please confirm bidder can adopt u-girder spans for 28m or alternatively bidder can proposed segmental box girder for 31m spans instead of 28m U-girders.	Superstructure U-Girder is the K-RIDE's preferred choice over box girder. In addition to this PSC I Girder with slab arrangements, steel I Girder with slab arrangement (Composite) and steel web Girders can be planned with approval of K-RIDE.
8	Section: 2 - instruction to Tenderers (ITT) Clause 3.3 c) - List of Minimum Key Technical Personnel Designation - Chief Safety and Health Manager Qualification - Bachelor' Degree in Civil Engineering & Diploma in Safety Course	Please revise as follows, Designation - Chief Safety and Health Manager Qualification - Bachelor' Degree in Civil Engineering/Any Degree & Diploma in Safety Course	The present tender condition prevails.
9	General NIT Bid submission date is 10/02/2022	We have received pre-bid replies and related addendum which we are scrutinising. We require at least 15 days extension in bid due date to incorporate changes as per pre-bid replies and related addendums. We request you please extend bid submission date by at least 15 days to enable us to submit our most competitive bid.	The bid submission date is amended as : Last Date and Time for receipt of Tenders Dated:21/02/2022, IST 15:00 Hrs. (Only electronic Tender permitted). Corrigendum-3 is uploaded for the same in Karnataka e-procurement portal and K-Ride website.

Running SI No:	Reference as per Tender Document	Tenderer's Query	Reply/Clarifications by K-RIDE
10	General	As per our understanding the existing track will be shift on newly constructed formation on one of new BSRP Line, kindly confirm that the train operation on existing track will be continue during the new track laying on new formation & dismantling of existing track will start only after diverting of rail traffic on newly constructed track on BSRP formation. Kindly also let us know the time period required for laying of new track on BSRP formation. Or Existing track will shift to the new formation with in a allowed block period, if so kindly intimate the months of block period for the shifting	The modus operandi is attached in Page no. 142-148 Annexure of tender document drawing in Pre-bid query and replies, same can be used as a reference. The elimination of level crossings works between Ch: 5.700 Km and 10.575 Km has involved raising of embankment, construction of minor bridges, RUBs of full length for accommodating both BSRP and IR tracks and retaining walls, sacrificial walls and the work will have to be carried out in stages for continuing the IR traffic by diverting through the newly constructed formation. The contractor to work out the methodology suitably for stagewise construction for continuing the IR traffic without any hinderances and this aspect may be considered while quoting the Lumpsum Price. Nothing extra shall be payable on for phase wise construction. The area below the IR track after diverting the existing IR traffic on proposed BSRP track by the respective contractors, will be handed over immediately and accordingly, the delay due to handing over of area below IR track on account of authority will be calculated and necessary Extension of Time shall be given as per the contract conditions. ✓
11	Section 8A: EMPLOYER'S REQUIREMENT – GENERAL INFORMATION AND SCOPE OF WORK, Annexure pb 02 f1 ,Annexure pb 02 f2,Annexure pb 02 f3 & C2 tender document PN 262 of 1349, the retaining walls: The retaining walls of 12.450Km is required to be construction and the approximate height is varying from 1m to 9m. for supporting the embankment/cutting slope and the details are in Tender drawing. The retaining wall of length approximately 7.2km has no surcharge (Slope) and remaining retaining wall length of 5.25 Km has surcharge (slope).	As per initial drawings sacrificial retaining wall shown from chainage BSRP 5.7 Km to 10.575 at various location but as per the latest drawing there is no sacrificial retaining wall consider for construction, Kindly confirm & furnish the reasoning behind the elimination of sacrificial retaining wall or let us know if there is any alternative method to be adopted in lieu of sacrificial retaining wall. Due to change in drawings the length of retaining wall decreases as compare to scope of work kindly furnish the revised qty.	These are tentative drawing and sacrificial retaining wall will be there between BSRP 5.7 km to 10.575 Km . The Sacrificial retaining wall is required in between chainage 5.700 Km to 10.575 Km in raising of formation and elimination of LCs. The Sacrificial retaining wall height is about 2 to 3 m with surcharge and retaining wall cross sections for 2 to 3m height may be refereed from the tender drawing at page no 151.The approximate length of Sacrificial retaining wall is about 1300m in between IR and BSRP tracks and nothing extra shall be payable for any variations and same are included in the lumpsum price. However, The updated tentative reference drawing are attached in Annexure PB-2A. ✓
12	Section 8A: EMPLOYER'S REQUIREMENT – GENERAL INFORMATION AND SCOPE OF WORK, Annexure pb 02 f1 ,Annexure pb 02 f2,Annexure pb 02 f3 & C2 tender document PN 262 of 1349, The RCC drains of 4.580Km (Approx.) is required in viaduct portion and 39.977Km is required in At-grade section (i. One line of 17.551Km on BSRP toe side, one line of 17.551Km between IR and BSRP track and one line of 4.875Km on IR toe side where elimination of LC's is involved. The minor variations in lengths may takes place during construction's. The retaining walls: The retaining walls of 12.450Km is required	Due to change in drawings the length of drain decreases as compare to scope of work kindly furnish the revised qty.	The RCC drain length will remain same . The RCC drains of 4.580Km (Approx.) is required in viaduct portion and 39.977Km is required in At-grade section (i. One line of 17.551Km on BSRP toe side, one line of 17.551Km between IR and BSRP track and one line of 4.875Km on IR toe side where elimination of LC's is involved. The minor variations in lengths may takes place during construction ✓
13	General Annexure pb 0 f1 ,Annexure pb 02 f2, Annexure pb 02 f3	The existing ground level are provided in latest drawings not matching with the old drawings at various chainages, kindly clarify	The latest uploaded drawings to be considered. However, all drawings are tentative only. Further details required if any to be assessed by the Tenderer. ✓
14	Pg No 159 of 203 at chainage Track overlapping adjacent track, Annexure pb 02 f2 	At Ch 215+000 centre to centre distance between track is only 3.93 meter & track is overlapping to each other, but as per standard code of practice it is not practically possible, kindly clarify.	The drawing is corrected and uploaded in Annexure PB-3A. The drawings are tentative. The required minimum centre to centre distance is already mentioned in tender document. ✓
15	General pg. no: 103 of 145, Annexure pb 02 f1	In some of drawings proposed track is out of existing railway boundary & not showing any details of proposed new ROW so bidder requests you to kindly furnish the chainage wise handing over date along with available ROW from date of commencement of project, this will require for our resource planning.	Wherever the proposed alignment is outside the existing railway boundary, in that stretch the alignment boundary is marked with green colour. The major portion of the required Railway land for the execution of works is available along the alignment. The remaining land or portion of land near to the Railway land (Private land if any) shall be acquired and handed over to the contractor progressively. The land acquisition is in progress for private land. The process for permission to work in railway land is in progress and the majority of railway land will be handed over progressively within 4 months (tentative) from date of LOA. The majority of private land will be made available within 8 months (tentative) from date of LOA progressively. It has been already clarified in PBQ dated 25.01.2022. ✓

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Running Sl No:	Reference as per Tender Document	Tenderer's Query	Reply/Clarifications by K-RIDE
16	Page no: 1,2,3,4 of 203 	Kindly furnish the drawing of existing track to be dismantled at following locations. Ch.5+925.00, Ch.5+950.00, Ch.5+975.00, Ch.6+000.00 As in drawings existing track not shown.	Corrected and enclosed as Annexure PB-4A. ✓
17	Page no: 81 of 233, Annexure Pb 02 f03 	Please refer attached figure of BSRP Ch 20.6 wherein the height of drain is more than 7 meters without considering any retaining structure construction of this much deep drain is not possible without any retaining arrangement kindly clarify. This one figure is only one example however the same type of drains are shown at various locations of at grade portion.	The retaining wall structure shall be constructed in all such locations. The drawing showing both retaining wall and drain is attached in Annexure PB-5A. The standard tentative drain cross sections are already given. ✓
18	Page no: 67 of 233, Annexure Pb 02 f03 	We understands that the rising/shifting of existing track works to be done between Ch 5.7 of 10.575 only. Please refer attached figure of BSRP Ch 20.250 wherein the proposed excavation infringing the existing alignment we think there is some mistake in drawing, kindly clarify.	corrected and enclosed as Annexure PB-6A. ✓
19	PART-2 EMPLOYER'S/WORK REQUIREMENT CLAUSE NO 2.3 ON PAGE NO. 393 OF SECTION-8A, Since the U- Girders are preferred in superstructure, the spacing of the tracks to be considered accordingly. The track spacing on viaduct of minimum 4.725m to be considered for Box girder/I-Girder with slab and 5.2m for U-Girders.	IN "PB - 12" SPAN ARRANGMENT HAS BEEN MENTIONED WITH 31.0M SPANS AND THE TRACK C/C SPACING IS MENTIONED IS 4.725M. U-GIRDER CAN NOT BE ACCOMMODATE AS PER THE THIS CLAUSE THEREFORE PLS CONFIRM CAN TRACK CENTRE TO BE REVISED FROM 4.725 TO 5.2M MINIMUM FOR U GIRDER. AND IF U-GIRDER IS THE PREFERRED CHOICE THEN EMPLOYER SHOULD PROVIDE THE U-GIRDER DRAWINGS (PT AND REINFORCEMENT) BECAUSE U-GIRDER IS SYSTRA'S PATENTED. PLEASE CLARIFY.	The present tender is EPC tender. The scope of design of Box girder/I-Girder with slab/open web girder / composite girder / U-Girder is in contractor's scope. However, the track centre can be changed to 5.2m for U-Girder. The span arrangements shown in GAD are tentative only.
20	Geo-Technical Investigation Details, CLAUSE NO. 5.3 , PAGE NO. 23, Recommended Pile capacities are given without considering the socketing depth	As per GTR Report, we are getting rock in all the bore holes. As per IRC 78, Pile capacity should be calculated a considering the socket side friction. However in Tender GTR Soil has been considered for calculation of pile capacity. Please clarify	The GTI report of tender document is based on preliminary investigation and it is tentative and for reference only. The tenderer has to assess further details as per prevailing standards.
21	PART-2 EMPLOYER'S/WORK REQUIREMENT, CLAUSE NO. 1.1 / A/ 8 ON PAGE NO. 263 OF Section 8A, The contract includes station piers @ approximate 15-22 m span in station portion.	AT LOTTEGOLAHALLI STATION SPAN IS 31.0M. PLEASE CLARIFY	The span arrangement is tentative. The revised drawings are uploaded in Annexure PB-7A. ✓
22	CORRIGENDUM (FIRST SET), REPLY NO. 502, APPROX. QUANTITY OF ROAD FOR DIVERSION IS DEFINES AS 80000 SQM. FOR VARIOUS SECTIONS AS BELOW: (1) VIADUCTS 24255 SQM. (2) EARTHWORK & RETAINING WALL SECTION 20300 SQM. (3) MINOR BRIDGE SECTION 14000 SQM. (4) MAJOR BRIDGE SECTION 1260 SQM. (5) RUB 14700 SQM. (6) ROB 5485 SQM.	KINDLY CLARIFY WHETHER ROAD CRUST REQUIRED TO CONSTRUCT ROB APPROACHES IS INCLUDED IN ABOVE TOTAL QTY. OF 80000 SQM.	Yes included.
23	CORRIGENDUM (FIRST SET), S. NO. 23 PARA (21) SECTION 8A PART-1/ PG.278, GEOTEXTILE LAYER BELOW BLANKET IS PROPOSED IN 14.2KM LENGTH OF FORMATION WIDTH.	LENGTH OF AT-GRADE SECTION IS AROUND 17.5 KM. PLEASE CONFIRM THAT IN BALANCE LENGTH OF AT-GRADE SECTION, GEOTEXTILE LAYER IS NOT REQUIRED.	It is confirmed that in balance length of At grade section geotextile layer is not required.
24	CORRIGENDUM 2, Last date & time for receipt of tenders, 10/02/2022 IST 15.00 Hrs	Since this is an EPC contract and we have received revised drawing & pre bid replies on 27/01/2022,our design team will have to work again on the basis on new drawings & required some more time to finalize the quantities/methodology & due to prevailing rising concern of covid in Bengaluru, detailed final site visit is not possible, bidder request you to kindly extend the bid submission date by at least 6 weeks.	The bid submission date is amended as : Last Date and Time for receipt of Tenders Dated:21/02/2022, IST 15:00 Hrs. (Only electronic Tender permitted). Corrigendum-3 is uploaded for the same in e-proc and K-Ride website.
25	Clause 7, Page No. 126, Section 5 : CONDITIONS OF CONTRACT(CC) AND SPECIAL CONDITIONS OF CONTRACT (SCC) Defect liability period will be ONE Year from the date of commercial operations of the Section/Corridor.	Bidder once again reiterates to delink the start date of Defect Liability period from the date of commercial operations since the Track works and S&T works will be done by other Contractor and there will not be any control on completion by Civil Contractor.	The present Tender condition prevails.

Running Sl No:	Reference as per Tender Document	Tenderer's Query	Reply/Clarifications by K-RIDE
26	Clause 37, Page 165 SECTION-7: PARTICULAR CONDITIONS OF CONTRACT (PCC) 5. Stage Payment for Works	Bidder understands that Invoice certification and Payment will be as follows: 1.80 % Amount – Recommendations within 10 Days and Payment within the next 10 Days of recommendations. 2.Balance 20% Amount – Certification within 15 Days of Bill submission and Payment within next 10 days of certification. Kindly confirm.	The present Tender condition prevails.
27	Corrigendum (1st Set)Sl. No: 47, 4.5 BSRP/Railway Stations on Corridor-2. The proposed station locations may change as per the site conditions, availability of land etc., during finalization of GAD after awarding the work. The tenderers may please note that nothing shall be paid extra for the change of station locations.	If the location of stations changes after awarding the work, the planned costing for that tender condition by the Contractor goes for a change. And this shall be considered as an Variation Item. Please Confirm.	The present tender condition prevails. No variation shall be considered for change of locations.
28	Corrigendum (2nd Set) Sl. No. 1, The pre cast piers and pier caps in viaduct portion required to be constructed for the viaduct structures up to 10m height (GL-RL=10m) preferably and there may be small variations in height of pre-cast piers. The final decision will be by K-Ride as per site requirement and site feasibility regarding height or pre-cast piers.	Due to the Restricted Availability of Casting Yard near the Site and considering the narrow access roads available for movement of construction vehicles, Precasting of Piers up to 10m Height (mandatory as per employer requirement) will call for Enormous Logistics Support. This will hamper the total project w.r.t the duration. The Contractor request to remove this condition & the method of construction shall be chosen based on the Site Condition.	The present Tender condition prevails.
29	Pre Bid Query and Replies Part I Sl. No. 1, The pre cast piers and pier caps in viaduct portion required to be constructed for the viaduct structures up to 10m height (GL-RL=10m) preferably and there may be small variations in height of pre-cast piers. The final decision will be by K-Ride as per site requirement and site feasibility regarding height or pre-cast piers.	Due to the Restricted Availability of Casting Yard near the Site and considering the narrow access roads available for movement of construction vehicles, Precasting of Pier caps (mandatory as per employer requirement) will call for Enormous Logistics Support. This will hamper the total project w.r.t the duration. The Contractor request to remove this condition & the method of construction shall be chosen based on the Site Condition.	The present Tender condition prevails.
30	Pre Bid Query and Replies Part I, Running Sl No: 137, Elevated Viaduct is designed for 17MT loading and at grade section, Minor bridges, Major Bridges and RUB's are to be designed for 25 MT Loading.	The Contractor assumes that All the Elevated Stations including Benniganahalli Station to be designed for 17MT loading condition. Please Confirm.	Please refer DBR of Viaduct and stations. However, 17MT is axle load only and other aspects of loading has to be considered as per DBR. ✓
31	Pre Bid Query and Replies Part IRunning Sl No: 331 The Shape of Substructure and Superstructure are as per the Tender Drawings and Further Details are in Tender.	1.As the U-Girder Superstructure shown in the Tender Drawings is having projections at the bottom slab which is Aesthetically Unpleasant & has Poor Drainage Capability, please confirm that the Contractor can change the Shape for better aesthetic & structural efficiency. 2. As the Circular Pier will be Aesthetically Pleasing than the Rectangular or Square Pier shown in the Tender Drawings. Please confirm that the Contractor can change the Pier Shape. 3.As the Pier Cap shown in the Tender Drawings is seems Blocky which is Aesthetically Unpleasant, please confirm that the Contractor can change the Shape for better aesthetic & structural efficiency.	The tender drawings are tentative. During detailed design stage, all the aspects will be examined and considered. The piers will be rectangular/square shape only. ✓
32	Pre Bid Query and Replies Part I, Schedules Cl: 4.3 " Precast RCC box for accommodating one BSRP track below ROB of size (5.5x6.75m height), barrel length 35m	Details of the existing ROB are required to provide the RUB below the ROB approach.	Details of ROB is given in PB 15 of pre-bid queries and replies dated: 25.01.2022. ✓
33	Pre Bid Query and Replies Part I, Running Sl No: 67, For ROB 410A, spans are 35m and 27m approximately with PSC girder or steel I girder with slab shall be used. b) For ROB 408B the spans are 30m approximately with PSC box girder or steel I Girder with slab(composite steel I Girder) shall be used.	Cross section of the Road are required. Kindly clarify the no of lanes and Footpath to be considered on both sides .	The tentative drawing showing the details is attached in Annexure PB-8A. ✓

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Running SI No:	Reference as per Tender Document	Tenderer's Query	Reply/Clarifications by K-RIDE
34	Pre Bid Query and Replies Part I, Running SI No: 116, Only standard RDSO design RCC boxes shall be constructed at all locations. In exceptional cases where it is not feasible to adopt RDSO design RCC boxes may be constructed with the approval of CBE and CB/C on case to case basis after recording the reasons. The tentative RUB drawings are attached in the tender drawing.	1. Proposed RCC box sizes for BSRP tracks does not match with standard RDSO designs. In this case, are RDSO standard designs applicable?. Kindly confirm. 2. Proposed RCC boxes for BSRP tracks are with higher skew (approx. 37 degrees). But standard RDSO designs only accounts for lesser skew. In this case, are RDSO designs applicable?. Kindly confirm.	Design of all non standard RCC Boxes/skew bridges has to be designed by contractor. RDSO is for straight tracks only. All the drawings are tentative. During finalisation of GAD, the contractor to consider all the aspects.
35	Pre Bid Query and Replies Part I, As per Prebid replies PB 11 drawing no.KRIDE/BSRP/BBMP/CR2/020-2021	Bidder requests the Employer to provide the details of the Proposed ROB rotary at Baiyyappanahalli.	The proposed Rotary ROB piers details affecting the BSRP alignment is already given in PB11 of pre-bid queries and replies uploaded on 25.01.2022.
36	Pre Bid Query and Replies Part I, As per Prebid replies PB 05 drawing no.KRIDE/BSRP/CR2/TD/MNB/04/03, 02, 01	1. Strengthening requirement due to overlapping of BSRP tracks over the existing IR structures is required. 2. Completion plan/As-Built drawings details of the existing IR structures are required.	1. Strengthening requirement due to overlapping of BSRP tracks over the existing IR structures is required and it is scope of EPC contractor. 2. The required Completion plan/As-Built drawings details of the existing IR structures are available with IR. However, interface with IR for the same is in the scope of successful tenderer. The available drawings are already enclosed in tender document.
37	General	Since this is being an EPC project, can bidder design the U-girder to any span as per design/construction feasibility. Kindly confirm.	Design of any structures are in the scope of EPC contractor with the approval of K-RIDE only.
38	General, Drawings for At-Grade Stations	Bidder request the Employer to provide the drawings for AT-grade Stations.	Please refer the tender GAD drawings for tentative station locations in tender drawings. The scope of the work in stations is up to formation level and details are in tender document.
39	General, Drawings for Railway Yards	Bidder request the Employer to provide the drawings for Railway Yards.	The Railway yard drawings are available with IR. However, interface with IR for same is in the scope of successful tenderer.
40	Pre Bid Query and Replies Part I, Running SI No: 302, 1) The embankment and cutting cross sections at every 25m interval indicating retaining wall is in attached wall is in attached tender drawings. The tenderer to work out the details from cross section drawings, 2) The embankment slope is 2:1 (2H : 1V) and cutting slope 1:1 (1H:1V) Approx.	Please specify any surcharge load to be considered.	The detailed design is in the scope of EPC contractor.
	Pre Bid Query and Replies Part I, Running SI No: 312, All the pile are to be tested with integrity and cross hole sonic tests. 75% integrity test and 25% cross hole sonic test (dynamic integrity test). Further details are in technical specification section 8B.	If cross hole & PIT is considered has integrity test, section "8.7.10 : Dynamic integrity test" to be deleted. Section 8.7.11 already covers on the cross hole sonic logging test.	The present tender condition prevails.
42	General Shape of U - Girder - Patent Rights	The type of Super Structure " U-Girder" is preferred choice by KRIDE. We understand that shape of Super Structure is patented by either M/s. SYSTRA or any other design consultants. We presume that the patent related fee will be beared by KRIDE and not by EPC Contractor. Kindly clarify	As design is in the scope of EPC contractor, the whole responsibility lies with the successful tenderer. The contractor to bear the Patent related fees.
43	SECTION-7: PARTICULAR CONDITIONS OF CONTRACT (PCC), page no 135. "Accepted Contract Amount" means the amount accepted in the Letter of Acceptance for the execution and completion of the Works and the remedying of any defects. In the Letter of Acceptance, the Accepted Contract Amount shall have two components i.e. (i) the base amount including GST (ii) GST component (calculated at the rate for works contract service as per GST Laws).		SECTION-7: PARTICULAR CONDITIONS OF CONTRACT (PCC), page no 135. "Accepted Contract Amount" means the amount accepted in the Letter of Acceptance for the execution and completion of the Works and the remedying of any defects. In the Letter of Acceptance, the Accepted Contract Amount shall have two components i.e. (i) the base amount including GST (ii) Deleted. iii) The rates quoted by the Contractor shall be deemed to be inclusive of the sales, GST and other taxes.(SECTION – 5: CONDITIONS OF CONTRACT(CC) AND SPECIAL CONDITIONS OF CONTRACT (SCC), Clause 39 at page no 117)
44	The Tenderer shall submit signed and stamped copy of one complete set of technical proposal documents uploaded in Karnataka Public Procurement Portal to K-Ride office within four working days of Bid submission date. The evaluation will however be based on the technical proposal documents uploaded in Karnataka Public Procurement Portal only.		The Tenderer shall submit signed and stamped copy of one complete set of technical proposal documents uploaded in Karnataka Public Procurement Portal to K-Ride office within four working days of Bid submission date. The evaluation will however be based on the technical proposal documents uploaded in Karnataka Public Procurement Portal only.